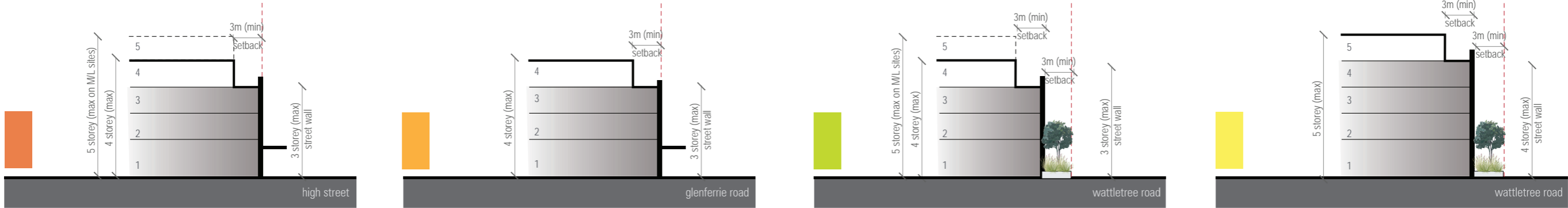
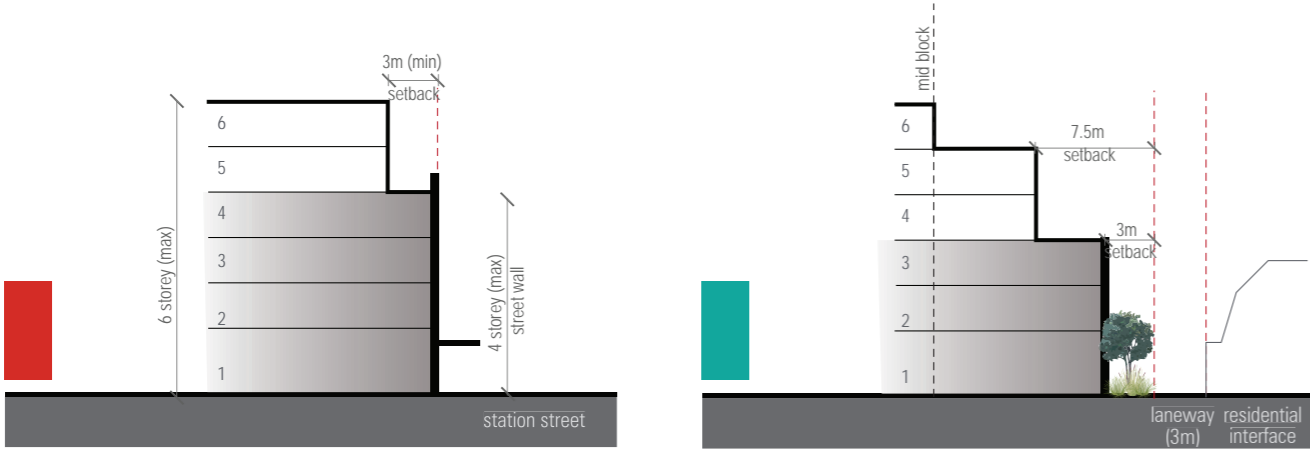


Typical Edge Condition

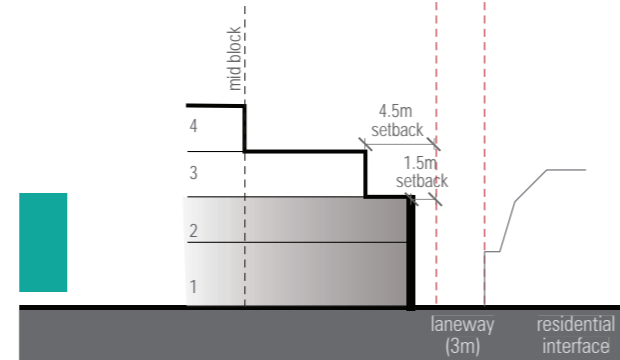
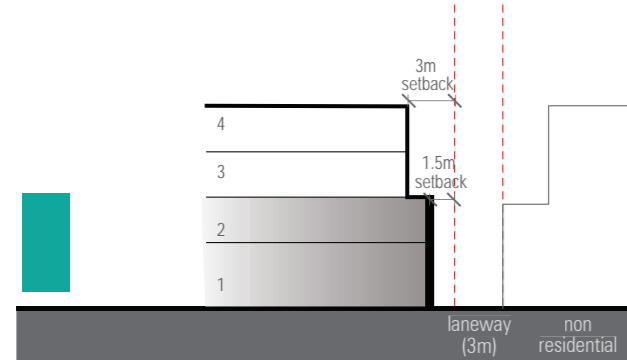
Main Spine Interfaces



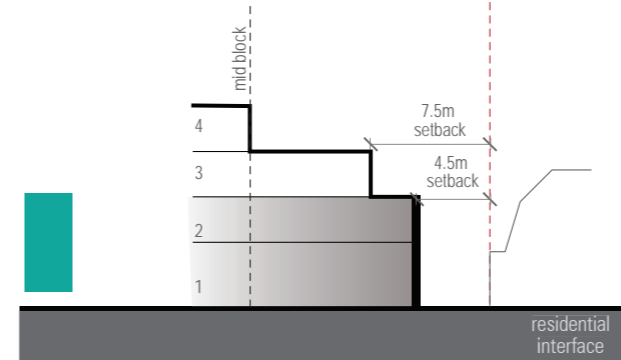
Highway / Rail reserve interface



Rear/ Laneway Interface



Rear Interface (no laneway)



Note:
Sensitive Residential Interface: sites with an immediate abuttal to a residential property (or adjacent along the rear laneway interface).
Non-Sensitive Interface: sites with an immediate abuttal non-residential property (or adjacent along the rear laneway interface).

Strategic development Site: Malvern Central

The preferred building form on the Malvern Central Strategic Development Site includes the following elements:

1. Maximum scale of 10 to 12 storeys at the north eastern corner, stepping down to 8 storeys to the south and west (when measured from the Wattletree Road frontage).
2. Separated slender tower forms with a maximum width of 25 metres.
3. Provide separation greater than 10 metres between potential tower forms on or off site.
4. A 2/3 storey street wall/ podium (equivalent to 3/4 storey residential) along Wattletree Road to maintain a consistent parapet line.
5. Preferred setback of 12 metres from the Wattletree Road parapet line to maintain Wattletree Road streetscape presentation.
6. Preferred setback to other external boundaries of 5 metres (minimum).
7. Any development of VicTrack land to match the future parapet line of Malvern Central's rear elevation.
8. No overshadowing of residential allotments to the south side of the railway line, or the footpath on the eastern side of Glenferrie Road should be caused at the equinox (22 September) between 9am and 3pm.

Building scale is required to be confirmed and determined through further site specific investigations and its positive net contribution to its context. Site specific investigations will require the preparation of a detailed urban form and public realm/ urban landscape analysis to determine an appropriate built form scale and public contribution.

Recommendations regarding built form scale for Malvern Central should clearly demonstrate how proposed built form is moderated by and respects the existing and proposed built form within the surrounding locality. Site specific investigations to inform built form scale must consider:

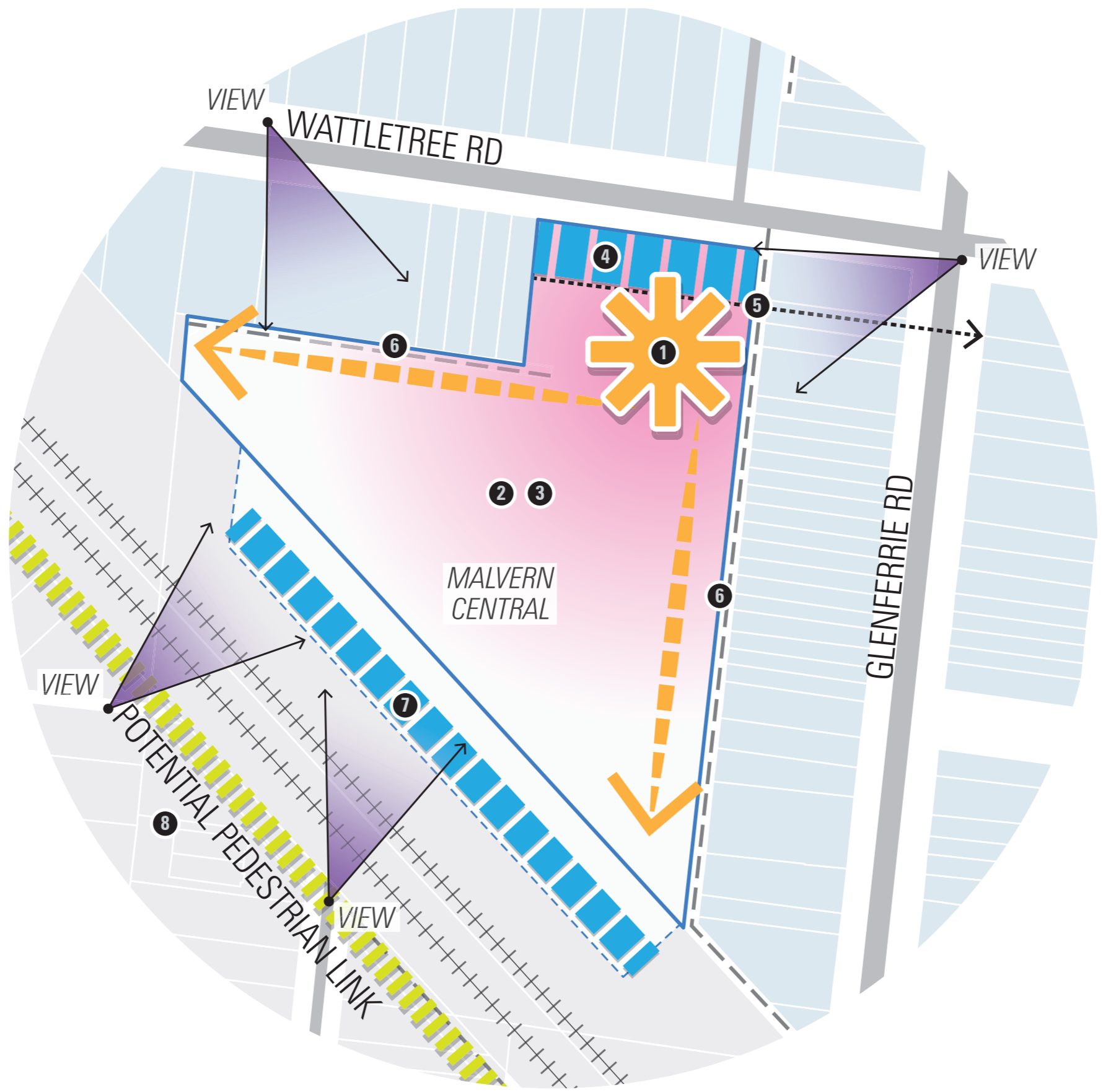
- Current built form (typology, height datum, grain and aspects) within 500 metres of the site.
- Built form recommendations as outlined within this Structure Plan, with particular reference to the 4 storey scale along Glenferrie Road and Wattletree Road.
- Built form scale associated with the preferred number of storeys to be guided by 4.0 m at ground level and 3.5 m for upper levels.

- The siting and orientation of taller built form to be centralised within the site to maintain prominence of heritage built form, particularly when viewed from Glenferrie Road.
- Immediate and longer range views of the site from public vantage points, including: Glenferrie Road, Wattletree Road and the rail corridor.
- Mitigation of potential visual bulk through separated towers with varied building heights, rather than horizontal setbacks alone.
- Opportunities to improve the southern elevation of Malvern Central and ensuring façade treatments address frontage interfaces and views to the site.
- Opportunities to improve pedestrian connectivity to Glenferrie Rd and between Malvern Central and Malvern Station.
- The utilisation of roof decks of lower forms to provide communal gathering space, urban landscape and a high quality outlook from upper level commercial or residential uses.
- Initiatives and objectives associated the Malvern Central Precinct Framework Plan (Figure 21).
- Development equity for abutting/ adjacent sites to the north and east through sufficient building separation (i.e. to maximise daylight access to lower floors, managing outlooks and external aspects, and privacy matter).
- An exemplary standard of internal amenity and avoiding reliance on privacy screening, as strategic development sites should accommodate amenity matters within its own site.
- Promotion of sustainability measures including: sustainable design, sustainable transport, minimising heat island effect etc.



Malvern Central from Glenferrie Road

Malvern Central built form diagram



Strategic Development Site: Dandenong Road Gateway

The preferred building form on the Dandenong Road Gateway Strategic Development Site includes the following elements:

1. The retention of the corner heritage form (Angel Tavern) in situ, without any upper level additions.
2. Preferred maximum building height of 8 storeys (when measured from the Dandenong Road frontage) including with appropriate design and management of built form at the interface the retained corner heritage form (Angel Tavern).
3. Ability to increase building height to 10 storeys with the provision of a 3 metres ground level setback of new buildings along north and south boundaries (so as to facilitate more generous ground level pedestrian space). 9th and 10th storey required to be setback to minimise overshadowing of Dandenong Road.
4. Preferred setback for upper levels above the podium is 5 metres (minimum).
5. Provide separation greater than 10 metres between potential tower forms on or off site.

Building scale is required to be confirmed and determined through further site specific investigations and its positive net contribution to its context. Site specific investigations will require the preparation of a detailed urban form and public realm/ urban landscape analysis to determine an appropriate built form scale and public contribution.

Recommendations regarding built form scale for Dandenong Road Gateway site should clearly demonstrate how proposed built form is moderated by and respects the existing and proposed built form within the surrounding locality. Site specific investigations to inform built form scale must consider:

- Current built form (typology, height datum, grain and aspects) within 500 metres of the site.
- Built form recommendations as outlined within this Structure Plan, with particular reference to the 4 storey scale along Glenferrie Road.
- Built form scale associated with the preferred number of storeys to be guided by 4.0 m at ground level and 3.5 m for upper levels.
- The siting and orientation of a slender vertical projection to be located away from the Dandenong Road and frontage to maintain prominence of the former Railway Hotel (Heritage Overlay Schedule 403).
- Potential taller form/s on this site should contribute positively to the skyline interest.
- A potential plot-ratio arrangement which balances additional floor space with ground level publicly accessible open space.
- Immediate and longer range views of the site from public vantage points, including: Dandenong Road, Glenferrie Road and Station Street.

- Ensuring potential façade treatments address frontage interfaces and views of the site, including opportunities to activate the northern façade along the pedestrian walkway to improve passive surveillance of the station environs and pedestrian connectivity to Malvern Station.
- Initiatives and objectives associated the Malvern Station Precinct Framework Plan (Figure 18).
- Development equity for abutting sites through sufficient building separation (i.e. to maximise daylight access to lower floors, managing outlooks and external aspects, and privacy matter).
- An exemplary standard of internal amenity and avoiding reliance on privacy screening, as strategic development sites should accommodate amenity matters within its own site.
- Promotion of sustainability measures including: sustainable design, sustainable transport, minimising heat island effect etc.

Strategic Development Site: Glenferrie Road Gateway

The preferred building form on the Glenferrie Road Gateway Strategic Development Site includes the following elements:

6. A podium form with a 3-4 storey base and preferred maximum building height of 8 storeys (when measured from the Glenferrie Road frontage).
7. Setback rising form behind the podium parapet line by at least 3 metres to the north and west, and at least by 5 metres to the south and east.
8. Position tallest form centrally within the site away from Glenferrie Road Railway Reserve junction and the adjacent heritage form (Angel Tavern).
9. Provide separation greater than 10 metres between potential tower forms on or off site.

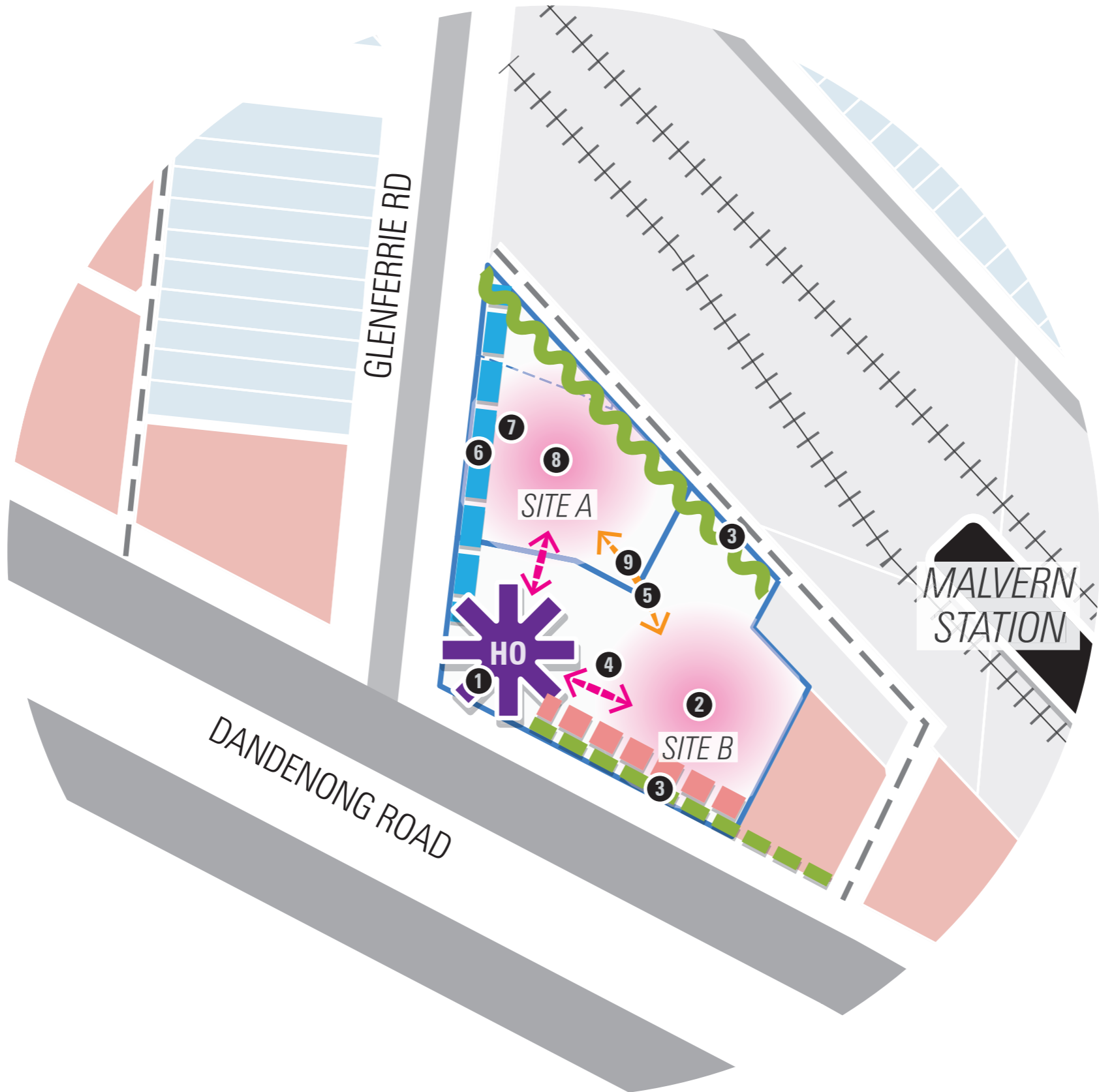
Building scale is required to be confirmed and determined through further site specific investigations and its positive net contribution to its context. Site specific investigations will require the preparation of a detailed urban form and public realm/ urban landscape analysis to determine an appropriate built form scale and public contribution.

Recommendations regarding built form scale for Glenferrie Road Gateway site should clearly demonstrate how proposed built form is moderated by and respects the existing and proposed built form within the surrounding locality. Site specific investigations to inform built form scale must consider:

- Current built form (typology, height datum, grain and aspects) within 500 metres of the site.
- Built form recommendations as outlined within this Structure Plan, with particular reference to the 4 storey scale along Glenferrie Road.

- Built form scale associated with the preferred number of storeys to be guided by 4.0 m at ground level and 3.5 m for upper levels.
- The siting and orientation of a slender vertical projection to be located centrally on the site to maintain prominence of the former Railway Hotel (Heritage Overlay Schedule 403).
- Potential taller form/s on this site should contribute positively to the skyline interest.
- A potential plot-ratio arrangement which balances additional floor space with ground level publicly accessible open space.
- Immediate and longer range views of the site from public vantage points, including: Dandenong Road, Glenferrie Road and Station Street.
- Ensuring potential façade treatments address frontage interfaces and views of the site, including opportunities to activate the northern façade along the pedestrian walkway to improve passive surveillance of the station environs and pedestrian connectivity to Malvern Station.
- Initiatives and objectives associated the Malvern Station Precinct Framework Plan (Figure 18).
- Development equity for abutting sites through sufficient building separation (i.e. to maximise daylight access to lower floors, managing outlooks and external aspects, and privacy matter).
- An exemplary standard of internal amenity and avoiding reliance on privacy screening, as strategic development sites should accommodate amenity matters within its own site.
- Promotion of sustainability measures including: sustainable design, sustainable transport, minimising heat island effect etc.

Dandenong Road and Glenferrie Road Gateway built form diagram



GLENFERRIE ROAD



ATTRIBUTES

(s) small sites are sites which have 6m street frontage (width), or less.

Glenferrie Road exhibits the highest level of consistency of small, fine-grained allotments due to its continued economic success since settlement. The spine between the rail cutting to the south and the Civic Precinct is perceived as a continuous set of 2 storey forms with pronounced parapets. As opposed to High Street where a greater degree of infill is anticipated, Glenferrie Road will emphasise the existing streetwall condition with any new built form set behind the parapet. New built form should seek to provide surveillance of rear laneways and incorporate softening of the laneway with tactile materiality and landscape elements where possible. Larger non-contributory forms within the street are to be carefully managed to complement existing built form in scale and vertical rhythm whilst avoiding heritage mimicry.



minimum typical site dimensions for a SMALL allotment on Glenferrie Road



GUIDELINES

- 1 maximum built form height of 4 storeys with 3 storey streetwall based on an appreciation of the established and predominantly street wall/heritage fabric, key views from the public realm within Glenferrie Road and side streets.
- 2 the preferred streetwall height of 3 storey (built to boundary) with top level setback minimum 3 m to ensure reduced visual prominence, whilst accepting a degree of visibility. For an existing building of heritage significance, refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated reference documents) for further guidance.
- 3 primary active frontages be maintained at the ground level of Glenferrie Road with attractive frontages to the rear lane incorporating tactile materiality, signage, lighting and low level softening vegetation in planter box format to provide a secondary address where possible.
- 4 building setbacks should be simplified to provide for good building composition and avoid a 'wedding cake' profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
- 5 a 1.5 m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 6 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
- 7 rear elevations should step down mid-block from 4 to 3 storeys where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- 8 above the first level, rear elevations should be setback a minimum of 3m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.

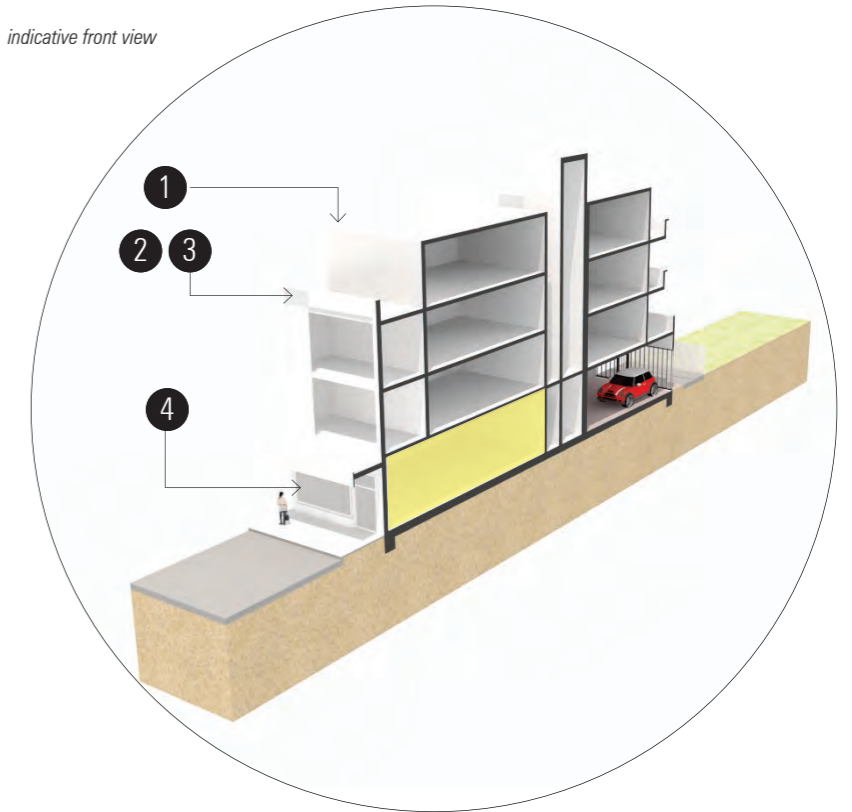


attractive rear interfaces with adequate design effects

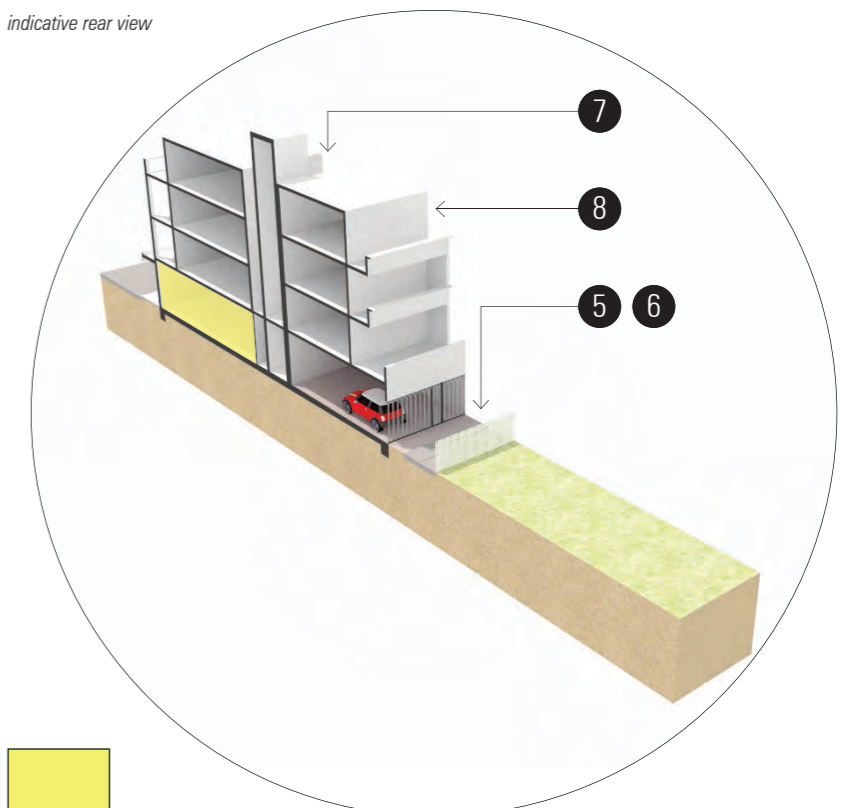


new infill building should reflect the existing parapet line

indicative front view



indicative rear view



encourage active uses at the ground level

GLENFERRIE ROAD



ATTRIBUTES

(m) medium sites are sites which have 6m-18m street frontage (width).
(l) large sites are sites which have street frontage (width) greater than 18m.

Whilst Glenferrie Road is dominated by fine-grained allotments a handful of heritage replica forms and non-contributory forms are noted which constitute medium or large allotments. These allotments include supermarkets, and mini-malls with arcades, and require positive management in terms of infill opportunity. As opposed to High Street where a departure in character on these larger sites is anticipated, new infill of larger allotments along Glenferrie Road will continue to emphasise the existing streetwall condition with any new built form set behind the parapet. New built form on larger sites should seek to provide a secondary address, with surveillance of rear laneways and incorporate softening of the laneway with tactile materiality and landscape elements where possible. Larger allotments should complement or instate an appropriate sense of vertical rhythm in ground and upper level treatments, whilst heritage mimicry should be avoided.



minimum typical site dimensions for a MEDIUM to LARGE allotment on Glenferrie Road



GUIDELINES

- 1 maximum built form height of 4 storeys with 3 storey streetwall based on an appreciation of the established and predominantly street wall/heritage fabric, key views from the public realm within Glenferrie Road and side streets.
- 2 the preferred streetwall height of 3 storey (built to boundary) with top level setback minimum 3m to ensure reduced visual prominence, whilst accepting a degree of visibility. For an existing building of heritage significance, refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated reference documents) for further guidance.
- 3 primary active frontages be maintained at the ground level of Glenferrie Road with attractive frontages to the rear lane incorporating tactile materiality, signage, lighting and low level softening vegetation in planter box format to provide a secondary address where possible.
- 4 building setbacks should be simplified to provide for good building composition and avoid a 'wedding cake' profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
- 5 a 1.5m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 6 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
- 7 rear elevations should step down mid-block from 4 to 3 storeys where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- 8 above the first level, rear elevations should be setback a minimum of 3m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.

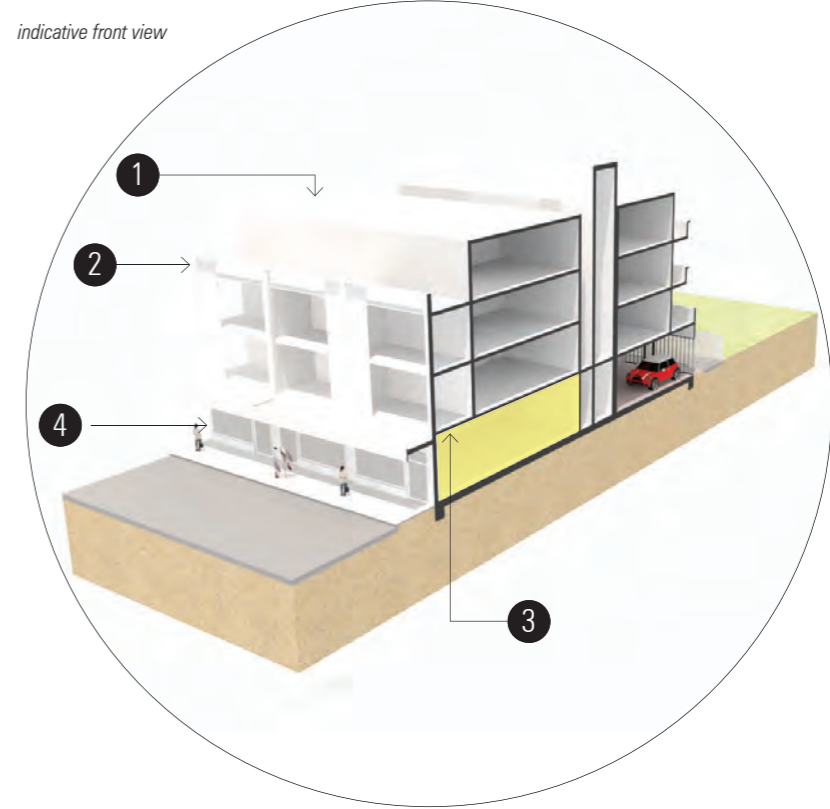


setback new built form beyond heritage fabric

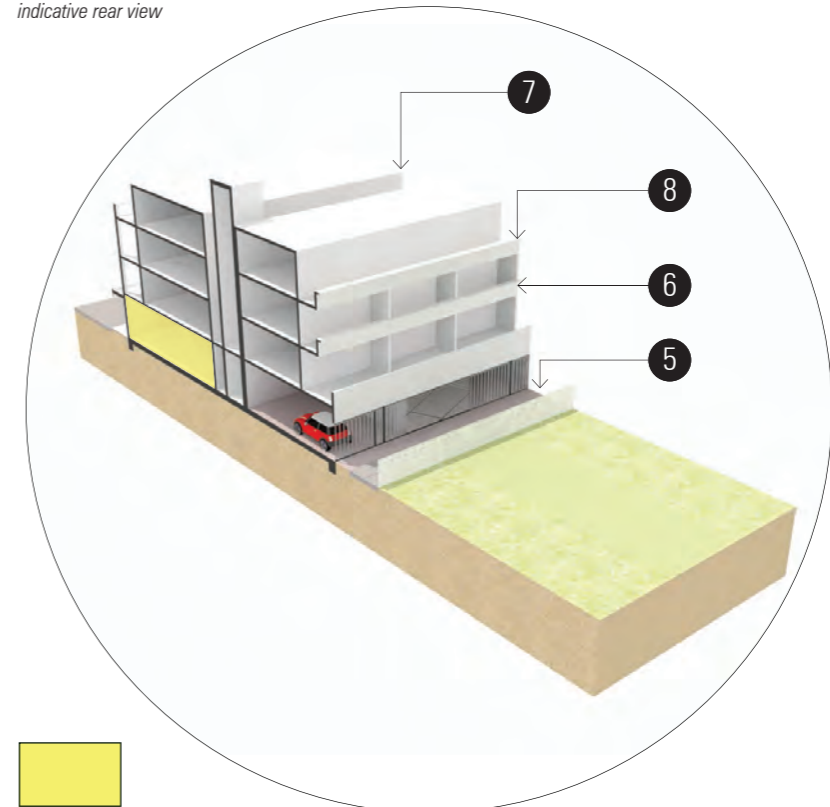


contemporary infill should achieve high standards of design

indicative front view



indicative rear view



encourage active uses at the ground level

HIGH ST



ATTRIBUTES

(s) small sites are sites which have 6m street frontage (width), or less.

High Street has a more varied character, influenced by a gently sloping street profile, varied parapet height and non-contributory buildings interspersed with valued period stock. New buildings should maintain and enhance streetscape diversity, with a combination of retained heritage fabric, and marginally taller contemporary streetwalls. Narrow and moderate sites can accommodate 4 levels. Where a sensitive residential interface exists to the rear, the building should step down in height by 1 level toward the rear boundary. New buildings should maintain the sense of openness to the sky through lightweight or retractable canopy treatments. All new apartments should be oriented with adequate spatial separation from adjoining development sites in accordance with equitable development principles. Apartments oriented with primary aspect to side boundaries should be discouraged.



minimum typical site dimensions for a SMALL allotment on High Street



GUIDELINES

- 1 maximum built form height of 4 storeys with 3 storey streetwall based on an appreciation of the established and predominantly street wall/ heritage fabric, key views from the public realm within High Street and side streets.
- 2 the preferred setback above the streetwall is 3 m to ensure reduced visual prominence, whilst accepting a degree of visibility. For an existing building of heritage significance, refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated reference documents) for further guidance.
- 3 building setbacks should be simplified to provide for good building composition and avoid a 'wedding cake' profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
- 4 primary active frontages be maintained at the ground level of High Street with attractive frontages to the rear lane incorporating tactile materiality, signage, lighting and low level softening vegetation in planter box format to provide a secondary address.
- 5 a 1.5 m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 6 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
- 7 rear elevations should step down mid-block from 4 to 3 storeys where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- 8 above the first level, rear elevations should be setback a minimum of 3.0 m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.

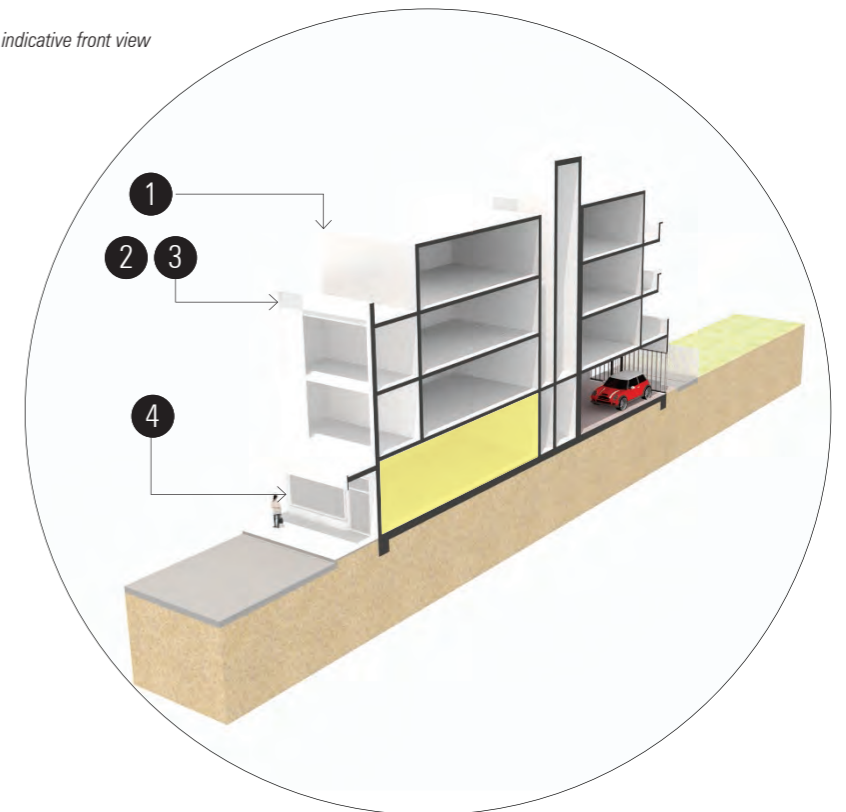


high quality architecture is permitted to project above the parapet line by 1 level in height

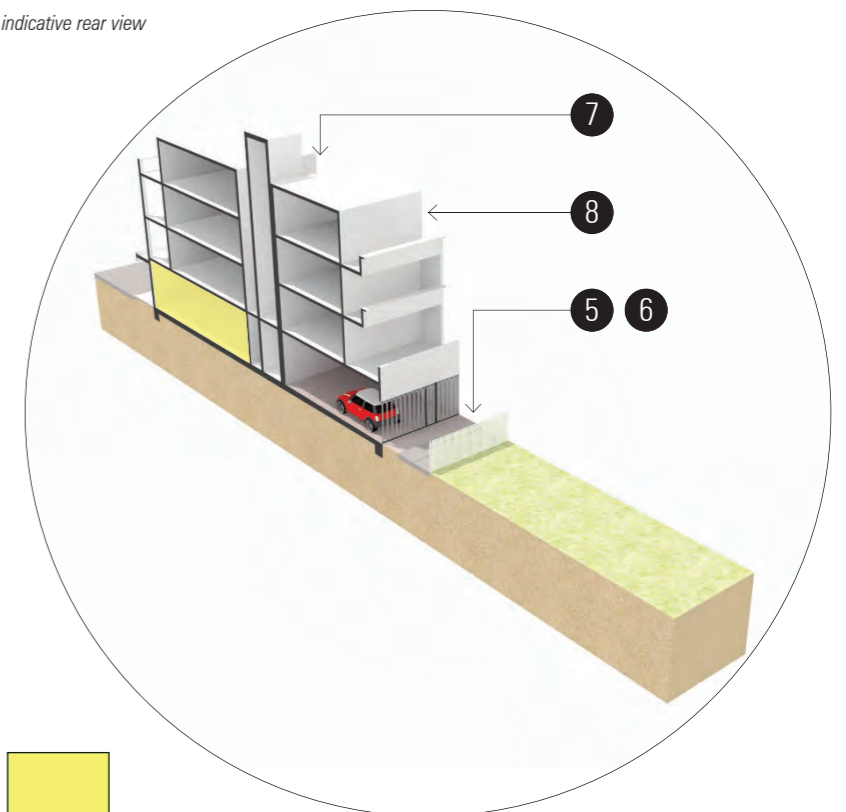


encourage party-walls with apartments oriented front and back wherever possible

indicative front view



indicative rear view



encourage active uses at the ground level

HIGH ST

M/L



ATTRIBUTES

(m) medium sites are sites which have 6m-18m street frontage (width).
 (l) large sites are sites which have street frontage (width) greater than 18m.

High Street has a more varied character, influenced by a gently sloping street profile, varied parapet height and non-contributory buildings intermingled with valued period stock. New built form on larger allotments should maintain and enhance the streetscape diversity, with a combination of retained heritage fabric, and marginally taller contemporary streetwalls. Larger allotments can accommodate a smaller 5th level envelope subject to the achievement of a high design quality. Where a sensitive residential interface exists to the rear, the building should step down in height by 1 level toward the rear boundary. New buildings should maintain the sense of openness to the sky through lightweight or retractable canopy treatments. All new apartments should be oriented with adequate spatial separation from adjoining development sites in accordance with equitable development principles. Apartments with primary aspect to side boundaries may be possible on larger allotments, so long as equitable development principles are adhered to.



minimum typical site dimensions for a MEDIUM to LARGE allotment on High Street

GUIDELINES

- 1 new building forms to incorporate existing heritage fabric where possible, with maximum built form height of up to 5 storey with 3 storey streetwall to maintain key views from the public realm within High Street and side streets. A 5th level element may be warranted if a high quality design and internal amenity standard is achieved on medium to large allotments.
- 2 preferred setback above the streetwall of 3m to ensure reduced visual prominence, whilst accepting a degree of visibility. For an existing building of heritage significance, refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated referenced documents) for further guidance.
- 3 building setbacks should be simplified to provide for good building composition and avoid a 'wedding cake' profile, whilst party walling should be maintained to at least 3 levels to encourage a strong street edge profile.
- 4 allow variation in the streetwall and make provision for a parapet height discrepancy of 1 level to achieve positive diversity, except where neighbouring heritage built form necessitates a site specific design response (refer to Clause 22.04 Heritage Policy of the Stonnington Planning Scheme (and associated reference documents) for further guidance.)
- 5 primary active frontages be maintained at the ground level of High Street with attractive frontages to the rear lane incorporating tactile materiality, signage, lighting and low level softening vegetation in planter box format to provide a secondary address.
- 6 a 1.5m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 7 rear building profile should incorporate sufficient setback to achieve the required building separation between building lines and primary outlooks.
- 8 rear elevations should step down mid-block 1 level in height where a sensitive interface occurs beyond the ROW (this does not apply to rear non-residential interfaces).
- 9 above the first level, rear elevations should be setback a minimum of 3.0 m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.

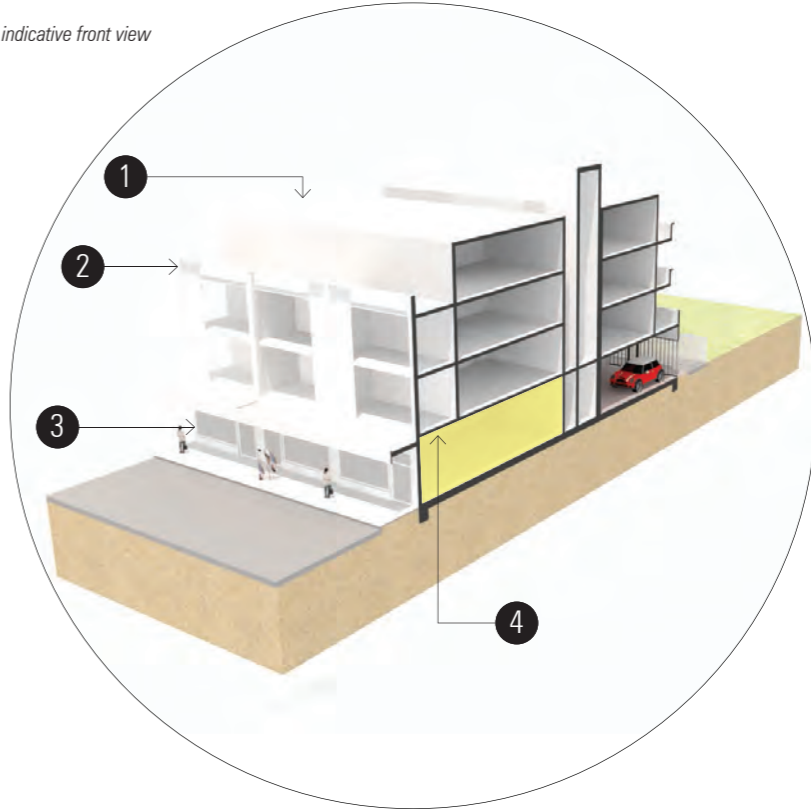


step the parapet form in height to add diversity

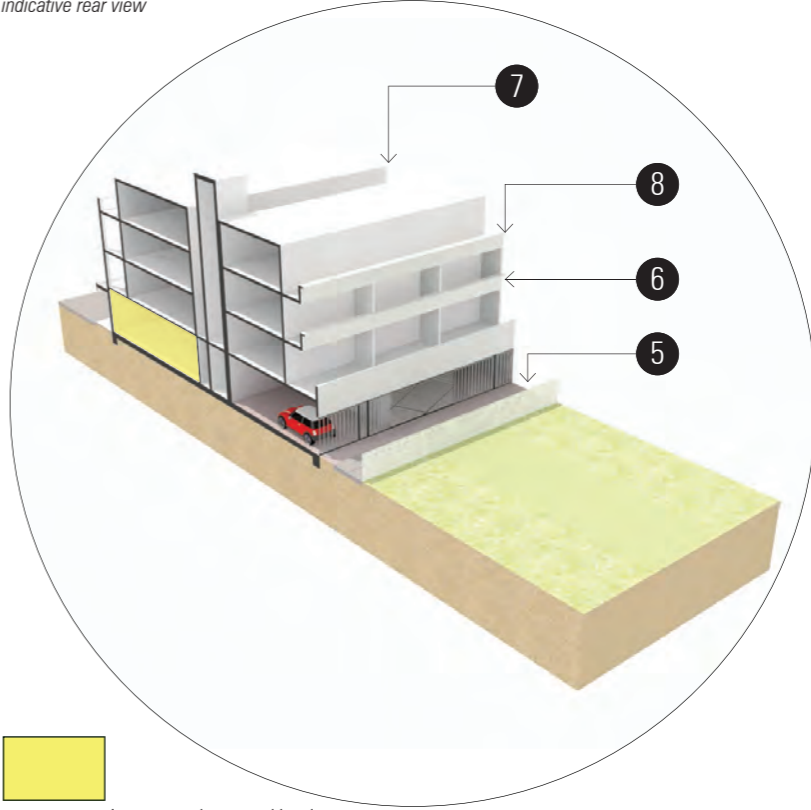


provide surveillance and a sense of address to laneways

indicative front view



indicative rear view



encourage active uses at the ground level

WATTLETREE WEST M/L

ATTRIBUTES

Applicable on all allotments situated along Wattletree Road, on the west side of Glenferrie Road.

An eclectic precinct dominated by the Art Deco Clocktower (on Wattletree Road) and the only street frontage of Malvern Central, this precinct comprises a mix of period dwellings, contemporary infill and commercial built form consistently serviced by a ROW to the rear. Notably the majority of dwellings remaining in the precinct are in use as offices or medical facilities, some in association with Cabrini. New built form in the precinct should seek to strengthen the street edge in a distinctive way given the lack of existing parapet datum, with taller streetwalls and party-wall construction, with buildings accommodating medical and office facilities in purpose built spaces, whilst maintaining adequate streetscape activation with foyer or common areas. New buildings of an urban profile should consider their temporary exposure over lower scale built form with adequate treatment of exposed walling.



minimum typical site dimensions for a MEDIUM to LARGE allotment on Wattletree Road West



GUIDELINES

- 1 building forms to adopt a 4 storey infill streetwall to promote surveillance as well as a 3m minimum setback to accommodate shrubs and small canopy trees within the frontage to contribute to the urban forest. A 5th level possible on larger sites subject to the achievement of high design and internal amenity standards.
- 2 facilitate a combination of landscaping and active frontage, including weather protection at entrance points along Wattletree Road.
- 3 all vehicle access or secure surface parking to be maintained off rear lane where possible, except on larger allotments where parking entry from Wattletree Road is necessitated for legibility purposes for patients or visitors to medical premises.
- 4 a 1.5 m minimum rear lane offset is required at the ground level and first level to facilitate the ongoing function of the laneway with all vehicle access from the rear lane.
- 5 above the first level, rear elevations should be setback a minimum of 3.0 m from the rear boundary for non-residential interfaces and 4.5 m from the rear boundary for residential interfaces. This is to manage building bulk, avoid a 'wedding cake' profile, and provide sufficient spatial separation to ensure good management of privacy outcomes. Balconies must be contained within their respective setbacks.
- 6 ensure that high levels of surveillance are achieved from the design of upper level apartments / tenancies to enhance the Wattletree Road street environment.

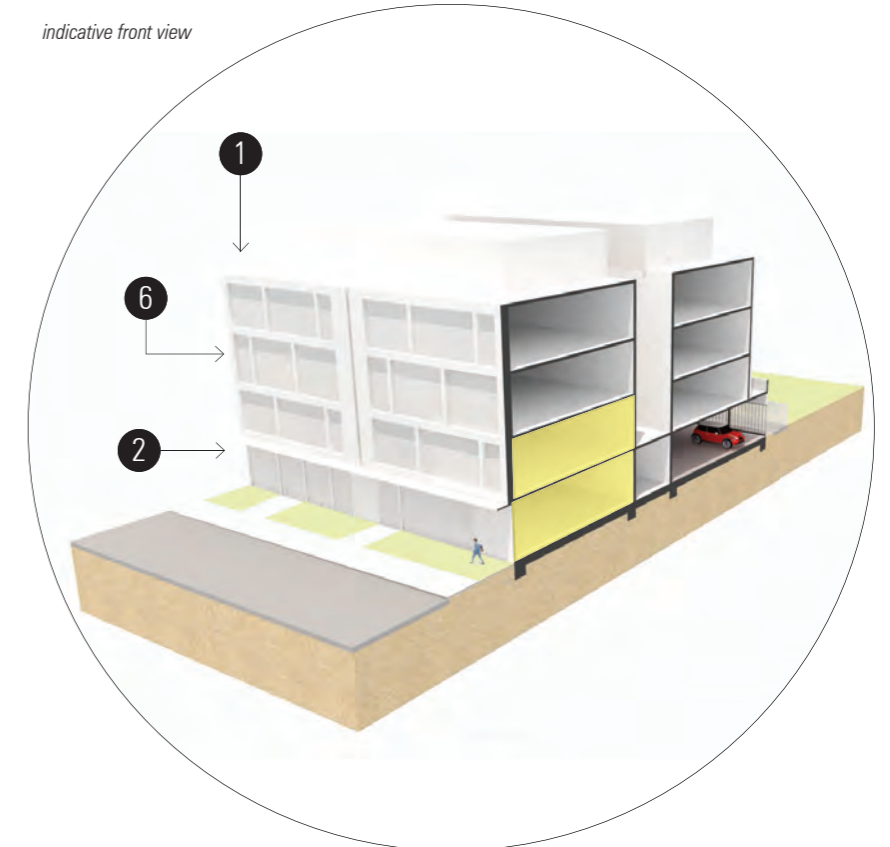


promote a taller streetwall to maximise surveillance

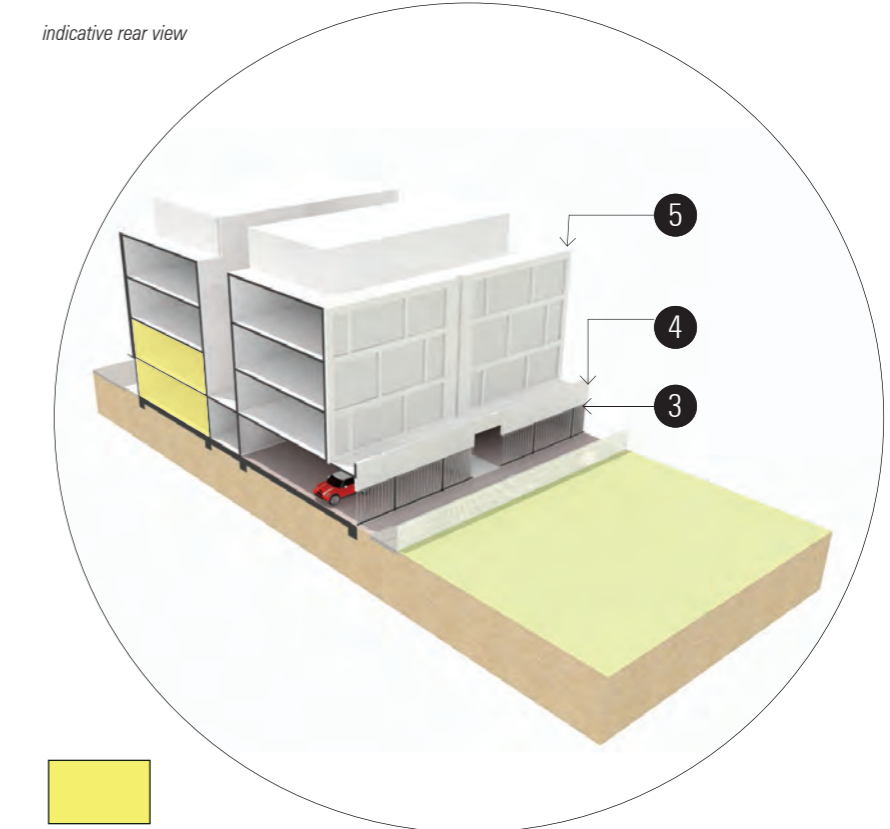


achieve a strong urban profile in new buildings

indicative front view



indicative rear view



encourage flexible street-oriented tenancies to enable a range of uses

WATTLETREE EAST M/L

ATTRIBUTES

Applicable on all allotments situated along Wattletree Road, on the east side of Glenferrie Road.

Wattletree East is distinctive from Wattletree West due to the dominant residential character and landscape quality within the Residential Growth Zone. Whilst some infill medical built form is noted, the primary stock is a combination of semi-detached dwellings in a garden and some multi-unit development. New built form in the precinct should seek to maintain the garden setting with more intensive built form sited off at least one side boundary with a garden lane effect, whilst small front and rear setbacks are to be maintained to accommodate small tree planting. Buildings will provide for adaptable ground levels which can accommodate either medical or office facilities, or home occupation components of maisonette dwellings.



minimum typical site dimensions for a MEDIUM to LARGE allotment on Wattletree Road East



GUIDELINES

- 1 building forms to adopt a 3 storey street elevation profile positioned with a 3m minimum setback to accommodate shrubs and small canopy trees within the frontage to contribute to the urban forest.
- 2 a recessed 4th level to be treated as a 'roof form' element and distinguished in materiality.
- 3 provision of a setback to one side boundary of at least 2.5m to create a garden lane effect and streetscape rhythm.
- 4 a rear setback should be provided of sufficient dimensions to enable the planting of at least one medium canopy tree abutting the rear lane
- 5 1.5m minimum rear lane offset is required to facilitate the ongoing function of the laneway with all vehicle access or secure surface parking accessed from the rear lane except where sites with a broad frontage (>18m) permit side access for legibility of medical facilities.
- 6 above ground level, rear elevations should be setback a minimum of 4.5m from the rear boundary to manage building bulk, avoid a wedding cake profile, and provide sufficient spatial separation to ensure good management of privacy outcomes.
- 7 activate ground level frontages through medical uses, or provide planter box buffers to ground level residences.

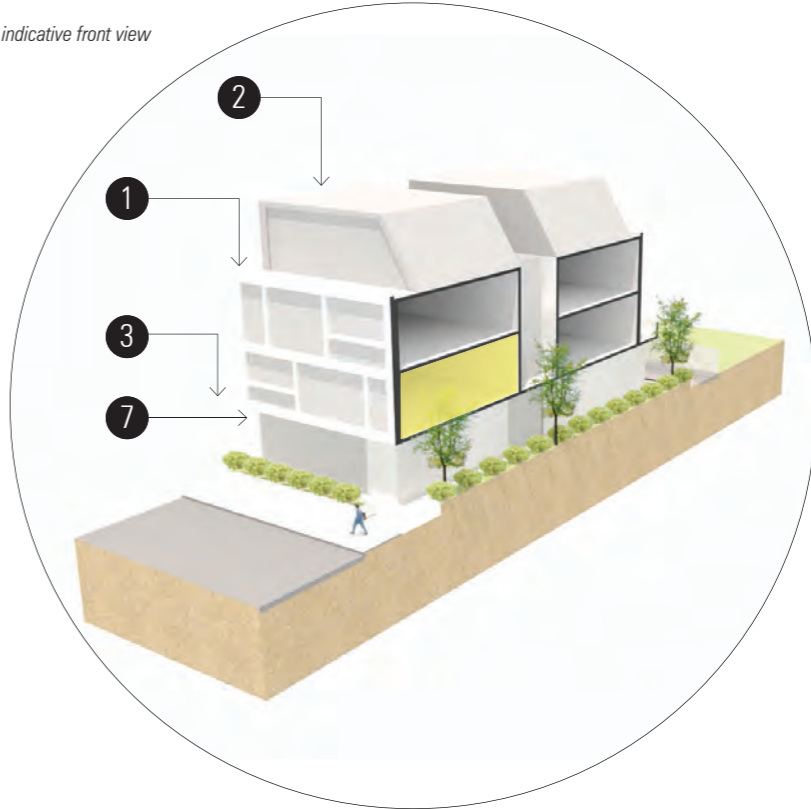


promote front setback with landscape treatments

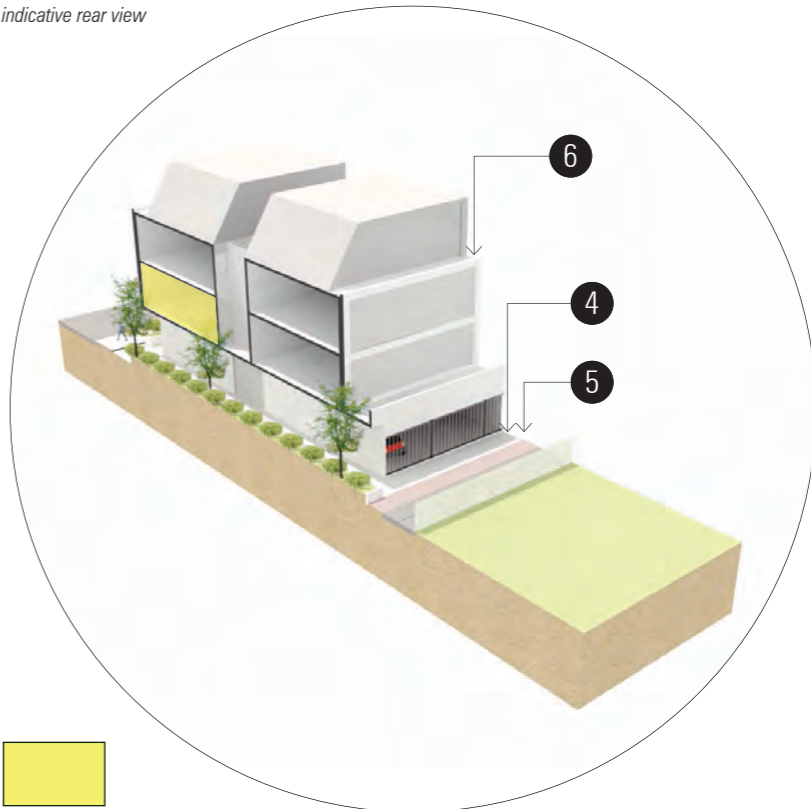


encourage recessed upper storey forms

indicative front view



indicative rear view



encourage flexible street-oriented tenancies to enable a range of uses

7 Neighbourhood Framework Plans

Precinct 1: Civic Centre



Figure 11 aerial context view



Malvern library



Terminal view along glenferrie road to malvern town hall



Malvern square adjacent to Giorgio's

Key Site Features

- Administrative and civic centre of the City of Stonnington;
- Existing function and image as the northern entry to the centre from the broad grain residential character of Glenferrie Road to the north;
- Significant heritage asset in Malvern Town Hall;
- Significant source of trips and custom in the school and other institutional buildings;
- Community orientated uses, including library, police station and public open space at Malvern Cricket Ground;
- Poor connectivity and image of back of house areas around the Malvern Cricket Ground; and
- Existing but poorly utilised public square at Malvern Square to the north western corner of Glenferrie Road and High Street junction.

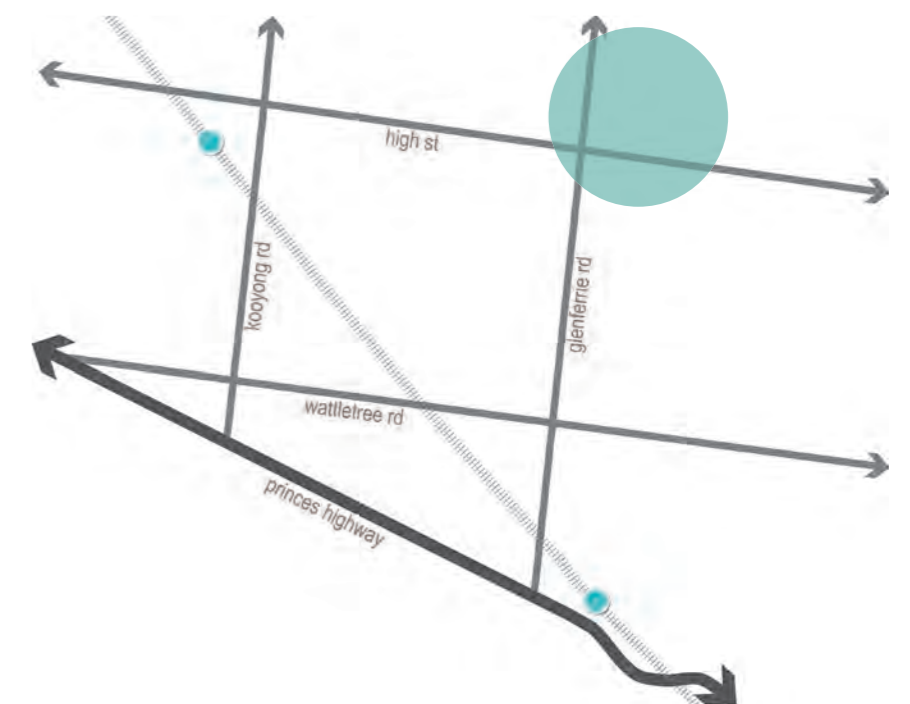
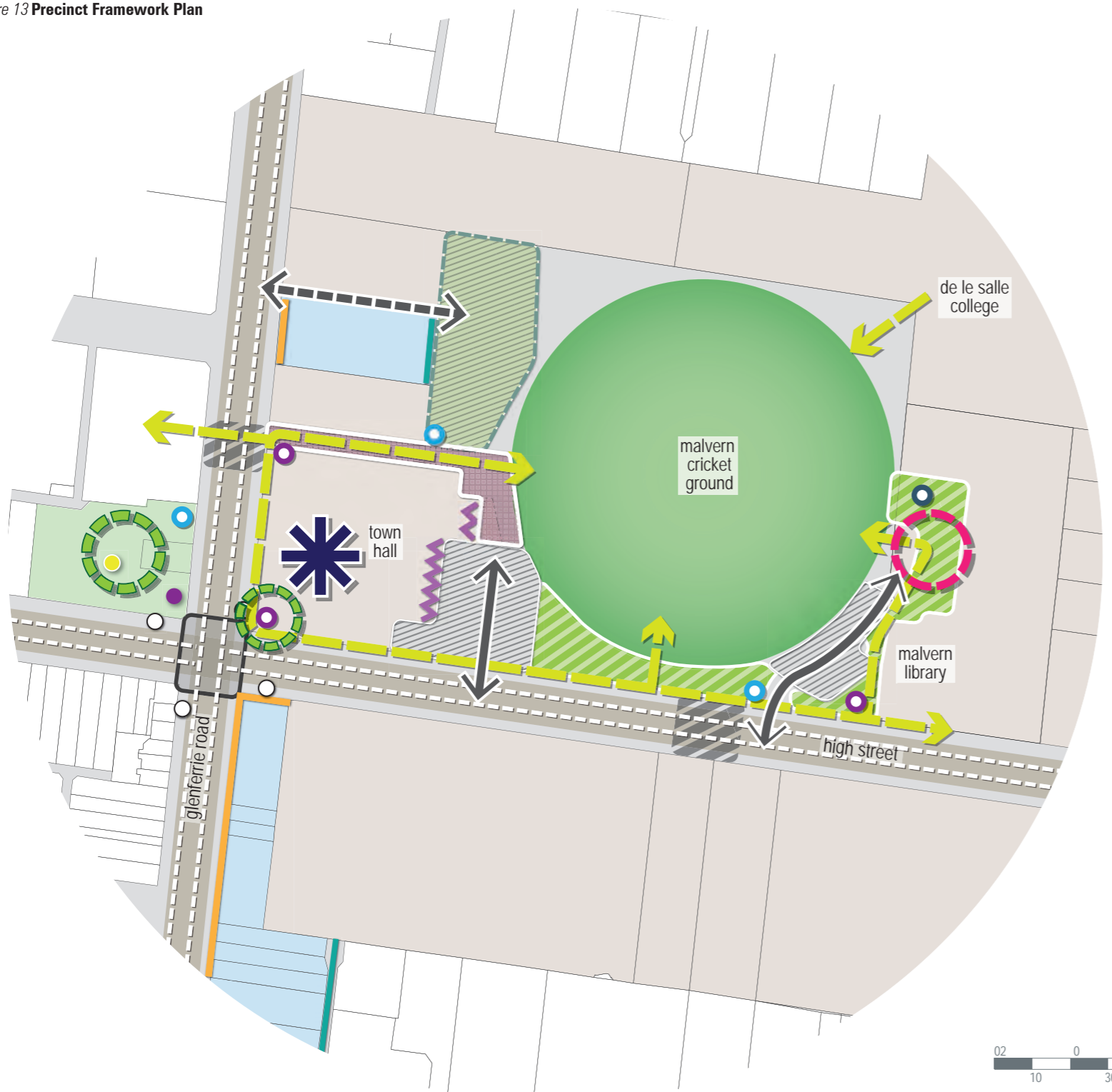


Figure 12 Precinct Identification

Figure 13 Precinct Framework Plan



Vision

The Civic Precinct will retain and strengthen its role as the central focus for municipal, community, educational and recreational activities in Stonnington. It will be a major destination attracting visitors from within and outside the municipality. As an integrated community and civic precinct set within an attractive landscape setting and supported by high quality public open space, the precinct will present itself as inviting to the local community, employees, customers and visitors alike.

Objectives

- to retain the primary focus as the municipal’s centre for civic services, education and recreation. Accommodate current and future requirements for Council and Community services and facilities in a manner which provides easy and direct access for the community;
- to recognise the existing Town Hall and Clocktower as a municipal landmark and to retain its role as a prominent built form in the Centre;
- to protect key vistas to Malvern Town Hall;
- to encourage future development around Malvern Cricket Ground to activate its interface and establish a safe and attractive environment;
- to encourage activities and linkages within the precinct and establish to be more closely integrated with the Town Hall, Malvern Square and Malvern Cricket Ground;
- to encourage activities and develop the existing Malvern Square as a premier public gathering space in the Centre;
- encourage the tactful siting of new public facilities within underutilised land between existing buildings reinforce the public realm and movement corridors;
- to strengthen the ‘leafy’ street character along the eastern side of High Street.

legend

main street	potential future vehicular access	glenferrie road interface	existing public art	street furniture hub
tram route	enhanced pedestrian connection	residential/ laneway interface	investigate landscape enhancement	way-finding/ public art
tram stop	preferred maximum building height 4 storey	improve interface presentation	potential pedestrian space	bike facility
existing signalised junction	building height subject to site specific investigation	existing landscaping	proposed shared path	
facillitate pedestrian crossing movements	existing car park	existing public open space	existing public space	
retain & improve vehicular access	existing landmark	existing public toilet	potential future activity node	

Precinct 2: Armadale Station



Figure 14 aerial context view



Internal view of kings arcade



The elaborate facade of kings arcade



Single storey period built form adjacent to the railway line

Key Site Features

- entry to the Centre from the west along High Street;
- proximity to rail, tram and bus services with a multi-modal opportunity;
- established boutique retail hub around Armadale Railway Station;
- existing Council surface car park off Morey Street;
- pedestrian access between Armadale Station and High Street via Kingsway Arcade; and
- characterised by back of house presentation and informal intimate streetscape setting.

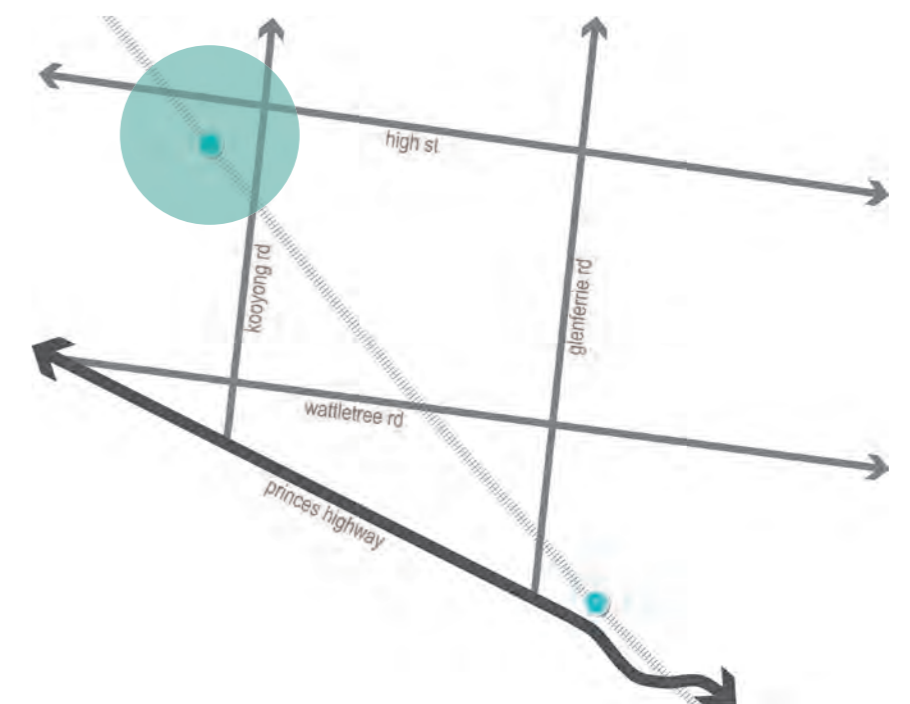


Figure 15 Precinct Identification

Figure 16 Precinct Framework Plan



Vision

Armadale Station Precinct will strengthen its role as a key attractor within the Centre and point of arrival. It will be experienced as a hidden 'gem' offering boutique retail experience complemented by high quality pedestrian realm. The Kingsway Arcade will retain its role to provide intimate connection to High Street and Kooyong Road with improved multi-modal linkages.

Objectives

- Enhance the legibility and image of the precinct as the western entry into the Centre;
- Bolster the identity of this precinct as a unique destination associated with Armadale Station and Kingsway Arcade;
- Establish high pedestrian amenity and gathering space around Armadale Station to facilitate safe access and to accommodate safe pedestrian refuge;
- Strengthen connections through the precinct to enhance the function of multi-modal links, connecting bus, train and tram services through public realm improvements;
- Facilitate north – south pedestrian movement through improved connections to and from the heritage Armadale Station railway bridge; and
- Carefully manage any new vehicle crossovers or building services from Morey Street to prioritise a pleasant pedestrian environment.

legend			
armadale railway station	tram stop	preferred maximum building height 4 storey	potential landscape enhancement
railway line (pakenham, cranbourne, frankston lines)	bus stop	preferred maximum building height 5 storey	proposed shared path
bridge	retain vehicular access	building height subject to site specific investigation	way-finding/ public art
main street	existing signalised junction	high street interface	way-finding/ public art
tram route	enhanced pedestrian connection	residential/ laneway interface	bike facility
bus route	potential future activity node	existing landscaping	

Precinct 3: Malvern Station



Figure 17 aerial context view



view down claremont avenue from station street



view from heritage rail bridge at malvern station



public realm along station street with median planting

Key Site Features

- Historic role as an entry to the Centre from Dandenong Road to the south;
- Distinctive high-quality streetscape along Claremont Avenue;
- Existence of higher built form along Station Street to the east of Malvern Station;
- Significant heritage station as a focal point of the precinct;
- Proximity to rail, tram and bus services;
- Prominent highway exposure to the south and more sensitive residential frontage to the north;
- underutilised public space and pedestrian access; and
- Degraded and poorly surveilled public realm south of the railway line and low quality street environment along Dandenong Road.

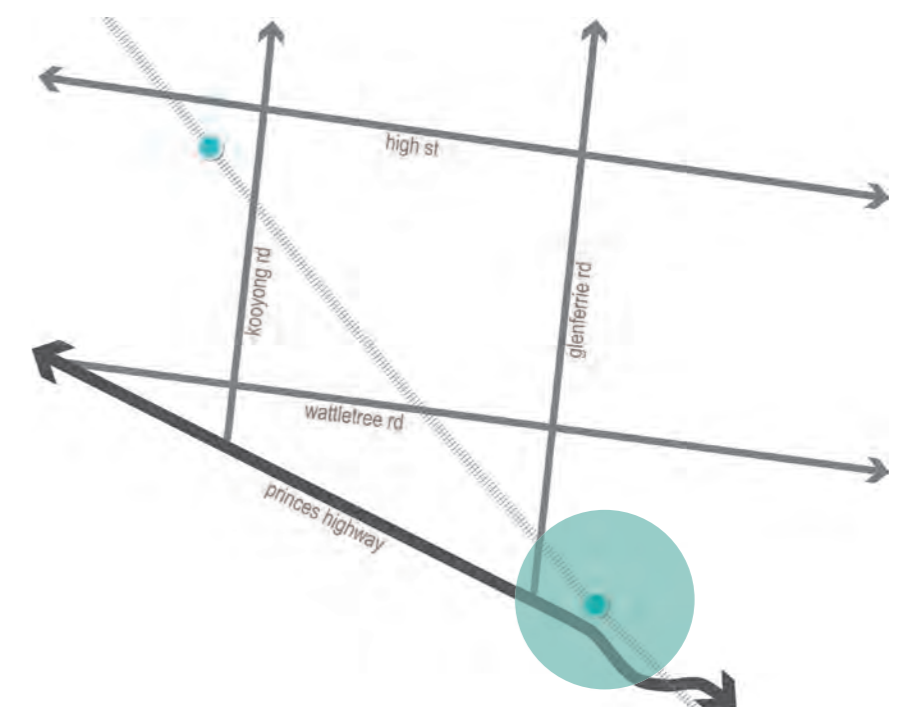
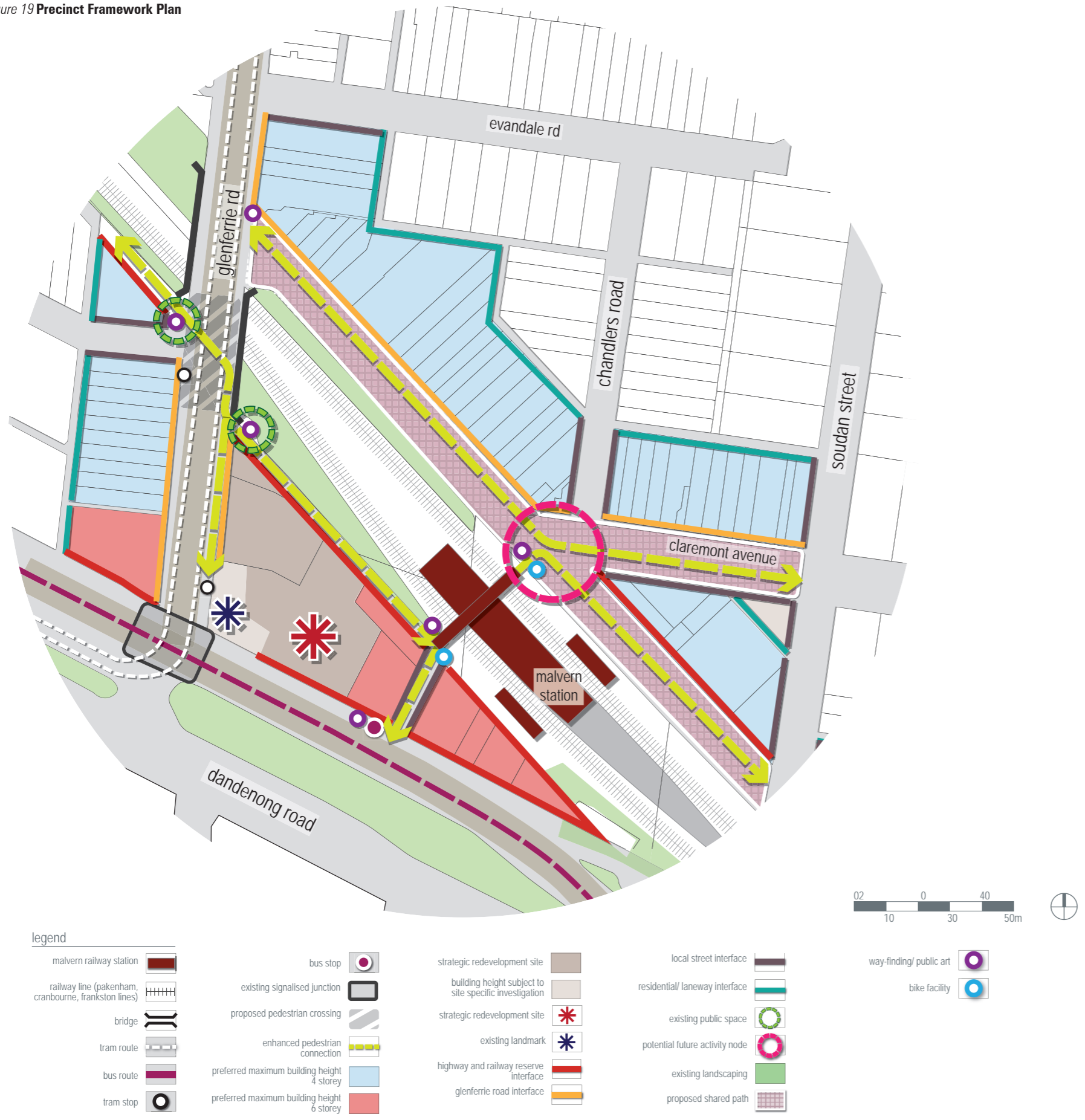


Figure 18 Precinct Identification

Figure 19 Precinct Framework Plan



Vision

This precinct will redefine the southern approach into the Activity Centre from Dandenong Road through its built form definition at the south-eastern corner of Glenferrie Road. It will promote the establishment of a commercial hub with capacity to accommodate higher density residential for properties fronting Dandenong Road. To the north of the railway line, a mixture of uses will remain in this area, with complementary retail and creative industries retained and expanded on. Pedestrian permeability will be enhanced with a better east – west connection across Glenferrie Road, as well as north- south connection across the railway line via Malvern Station. A new-shared space around Malvern Station seeks to establish a consistent pedestrian friendly environment connecting Glenferrie Road to the Station.

Objectives

- Enhance the image of the precinct as the southern of the Centre and maintain its role in supporting the highest intensity of commercial and mixed use activity;
 - Establish a strong sense of arrival into the Centre from Dandenong Road with the south eastern corner providing a strategic redevelopment site ;
 - Retain individually significant heritage buildings and integrate intensive infill development where possible;
 - Encourage active ground floor uses with potential pedestrian access along the southern side of the railway reserve;
 - Clearly define desired pedestrian line of movement for pedestrian and cyclists between tram stops, railway station and key destinations, including Malvern Central and Cabrini Hospital;
 - Maximise connections to the north – south pedestrian connection through the Railway Station;
 - Establish high pedestrian amenity and a publicly accessible meeting/ gathering space as part of an 'activity node' to the north of Malvern Station to connect to the Station Street and Claremont Avenue; and
 - Direct higher intensity built form to the south along Dandenong Road with a scale transition to more modest forms to the north, responding to the existing residential interface; and
1. Support the establishment of a 'creative' hub centred around Claremont Street and Station Street, including potential street market or special events.

Precinct 4: Malvern Central



Figure 20 aerial context view



Landmark art Deco clocktower on wattletree road



Positive street integration of malvern central at wattletree road



Laneway interface between malvern central and glenferrie road shops

Key Site Features

- Significant destination within the Centre, with Malvern Central providing a significant retail offering;
- Strong association with Cabrini Hospital to the east and the Wattletree Road spine;
- Poor integration, connectivity and legibility between the precinct and Malvern Station;
- Broad underutilised public depot land adjacent to the rail alignment;
- Car dominated environment due to the arrangement of ramping vehicle circulation into Malvern Central; and
- Mixed built form image along Wattletree Road, clearly distinct from the consistent character of Glenferrie Road.



Figure 21 Precinct Identification